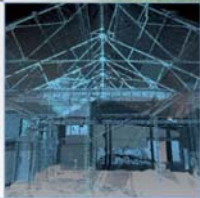


Maryhill Regeneration Stage 3, Archaeological Works: Data Structure Report

AOC 20677

April 2009



AOC
Archaeology
Group



ARCHAEOLOGY

HERITAGE

CONSERVATION

Maryhill Regeneration, Stage 3, Archaeological Works: Data Structure Report

On Behalf of: British Waterways Scotland
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Glasgow
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National Grid Reference (NGR): NS 5641 6910

AOC Project No: 20677

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Abstract

This report documents the results of a programme of archaeological works undertaken by AOC Archaeology Group, which included monitoring of ground-breaking works, evaluation and historic building recording in advance of regeneration works at Maryhill Locks, Glasgow, part of the Forth and Clyde Canal, a Scheduled Ancient Monument. The works consisted of the monitoring of five short trenches for bench foundations and the excavation of four evaluation trenches in advance of landscaping works. A historic building recording exercise was also carried out on the northern and eastern boundary walls of Kelvin Docks and of the upstanding remains in their vicinity in advance of landscaping. Two or three phases of slipway construction associated with the Kelvin Docks were identified. A concrete platform was also recorded in the northeast corner of the site.

1 Introduction

1.1 Background

- 1.1.1 AOC Archaeology Group was commissioned by British Waterways Scotland to undertake a programme of archaeological works at Maryhill, Glasgow (centred on NGR: NS 5641 6910). The archaeological works comprised the monitoring of ground-breaking works associated with the regeneration works, alongside archive consultation, Historic Building Recording and intrusive Evaluation trenching (Figure 1). The archaeological works were designed to satisfy the requirements of Historic Scotland due to the status of the Forth and Clyde Canal as a designated Scheduled Ancient Monument (SAM 6773).
- 1.1.2 The archaeological works covered all ground works associated with Stage 3 of the Maryhill Regeneration. These comprised the provision of seating areas to the south of the canal towpath and landscaping of ground between the Dry Dock and Maryhill Road/Skaethorn Road junction. Groundbreaking associated with the seating areas was archaeologically monitored (five trenches, each approximately 4.2 m in length), while a cartographic and photographic archive consultation to investigate the significance of the northern and eastern walls at the Maryhill Road/Skaethorn Road junction was followed by the excavation of four slot trenches (between 4.0 and 5.6 m long) across the access ramp on the corner of Maryhill Road and Skaethorn Road, to assess the deposits and their significance prior to any landscaping works being undertaken. A programme of Historic Building Recording was also conducted on the northern and eastern walls. The works were conducted under the terms of a *Written Scheme of Investigation* (AOC 2008), approved by Historic Scotland and under Scheduled Monument Consent. The works were carried out in accordance with the principals inherent in NPPG 5 (SOEnd 1994) and PAN 42 (SOEnd 1994a). The fieldwork was undertaken between the 24th and 25th March 2009, in variable weather conditions.

1.2 Location

- 1.2.1 The archaeological works were undertaken at Maryhill Docks on the Forth and Clyde Canal, located within the Maryhill district of the City of Glasgow (centred on NS 5641 6910; Figure 1). The proposed development is in a built up urban area with several waterways running through it; the River Kelvin and the man-made Forth and Clyde Canal. The topography of the area slopes steeply from east to west along the line of the canal. Land north of the canal slopes gently in a north direction. The canal is bounded on both sides by raised embankments. The area is located in a region which is cut by coal seams, ironstone beds and fireclays which would have provided the stimulus for the rise of industry on the site. Glasgow lies on part of the downfaulted rift which forms the Midland Valley of Scotland. This has left the solid geology of Carboniferous Limestone Coal formation. The drift geology overlying the Limestone consists mostly of Devonian glacial till (Hall et al 1998).

1.3 Archaeological Background

- 1.3.1 A Desk-Based Assessment was completed on the Maryhill Locks and the immediate surrounding area owned by British Waterways (Fouracre 2007), from which the following brief background to the historical development of the site is largely derived. A Desk-Based Assessment for the present works has also been produced (Martilla 2009).
- 1.3.2 From 1787-90 the Kelvin Aqueduct was constructed to allow the canal to flow over the River Kelvin. Not only was the aqueduct an integral part of the canal construction project but it also heralded a dramatic change for the small industrial district of Maryhill. At the time of its construction, the

aqueduct was the largest of its kind in Britain and as such it attracted much attention (Brown 1997). Indeed the aqueduct and associated complex of Maryhill Locks and Kelvin Dry Dock became an important tourist attraction as one of the biggest and finest examples of canal construction in Scotland. The Kelvin Dry Dock having been built in 1789 constitutes the Forth and Clyde Canal's oldest building yard (Brown 1997). The Maryhill Locks themselves comprise a flight of five locks at the western end of the summit level of the canal. Between each of the locks is a large oval basin with masonry walls. There is a fifth basin between the bottom lock and the Kelvin Aqueduct (Hume 1974). The purpose of the basins was to permit boats to pass each other whilst going through the series of Locks (Brown 1997). The regeneration works are planned to occur mostly to the east of the Kelvin Dry Dock and on the southern banks of the Maryhill Locks.

- 1.3.3 Prior to the present works, a site visit was made in January 2009. The Maryhill Locks complex was seen to comprise five restored timber locks linked by four oval basins with an elongated basin in the west linking the locks to the aqueduct (Plate 1). The basins were masonry lined and in a good state of repair. The vegetation either side of the locks had been recently cut and the tow path functioned as part of the River Kelvin walkway. The dry dock and slipway of the old Kelvin Dock are situated on the north side of the basin between Locks 22 and 23. The area west of the locks, where the proposed benches were to be located, was inspected; no archaeological remains were identified. The area north and east of the Kelvin Dock was examined. The remains of the slipway and a path were visible east of the dock (Plate 2). A mound of unknown nature was identified north of the dock (Plate 3). A wall north of the dock, subject to proposed removal, was inspected and photographed (Plates 4 and 5). Several phases of construction/repairs were identified. The eastern wall forming a boundary with Maryhill Road was also inspected and photographed.

2 Objectives

- 2.1 The objectives of this work were:
- i)* to record the character, condition, extent, quality and date of any as yet undiscovered archaeological remains within the proposed development area;
 - ii)* to use these regeneration and landscaping works alongside some additional combination of hand dug and machine excavated trenching to explore and assess the remains that exist to the east of the Dry Dock;
 - iii)* should significant archaeological deposits be discovered that cannot be preserved *in situ*, to prepare and implement an appropriate mitigation strategy;
 - iv)* to interpret the remains found and present them in their archaeological and historic context.

3 Methodology

- 3.1 The archaeological works included the monitoring by an experienced archaeologist of all ground breaking works associated with the provision of seating areas to the south of the towpath on the Forth and Clyde Canal. The groundbreaking was undertaken by a tracked minidigger with a ditching bucket, under archaeological supervision. Five trenches, each approximately 4.2 m in length were excavated and recorded in accordance with AOC Archaeology's standard practice and Historic Scotland guidelines. A comprehensive cartographic and photographic archive consultation informed the exact placement of each of the five proposed bench seats to ensure minimum potential for the disturbance of known buildings.
- 3.2 Machine excavation was undertaken in shallow units/spits and ceased on the first significant archaeological horizon, natural subsoil and/or maximum depth of disturbance, whichever was encountered first. Spoil was scanned for artefacts, both visually and by metal detector. A full and accurate record was made of all archaeological deposits and features.
- 3.3 The proposed landscaping of ground between the Dry Dock and the Maryhill Road/Skaethorn Road junction led to an initial comprehensive cartographic and photographic archive consultation to establish the significance of the northern wall (facing onto Skaethorn Road) and to establish the potential remains present in front of and against the eastern wall (facing onto Maryhill Road). Four trenches, measuring between 4.0 and 5.6 m in length and 1.6 m to 2.4 m wide, were excavated by a combination of machine (minidigger with ditching bucket under archaeological supervision) and hand excavation across the access ramp (from the corner of Maryhill Road and Skaethorn Road) to assess the deposits and their significance prior to any landscaping works being undertaken. All trenches were fully recorded: all significant archaeological features were cleaned and fully defined and recorded.
- 3.4 A programme of Historic Building Recording was conducted on the northern and eastern walls at the Maryhill Road/ Skaethorn Road junction and on *in situ* upstanding brick building remains in order to provide a comprehensive record of these remains prior to their being covered. The Historic Building Recording included a full photographic record of the eastern and northern walls and associated brick and concrete structures and a plan survey of brick and concrete structures with a Leica TCR 705.
- 3.5 A black and white print and digital photographic record of the fieldwork was taken along with a selection of general shots of the area (Appendix 2).
- 3.6 An abstract of this report will be presented in the Discovery and Excavation in Scotland publication (Appendix 2) and entered on the OASIS internet based pages.

4 Results

4.1 Watching Brief

- 4.1.1 The watching brief monitored the excavation of five almost identical trenches for bench foundations on the western side of Maryhill Locks (Figure 1). The deposits identified during the excavation comprised redeposited materials, most likely related either to the construction of a modern footpath or the tow path of the canal.

- 4.1.2 Trench 1 (Plate 6) measured circa 4.4 m east/west and circa 1.0 m wide at its east end and 1.4 m wide at its west end. It was 0.3 m deep. A 0.2 m deep slot trench, which measured 0.55 m north/south by 1.2 m east/west, was excavated for the bench foundations approximately 0.9 metres west of the eastern end of the trench. A 0.25 m wide concrete foundation [102] for a tarmac path [101] ran along the trench's northern edge. A 0.2 m wide blaise deposit [103] was located west of the concrete. The rest of the trench was covered by a dark brown loam [104] deposit, which was not fully excavated.
- 4.1.3 Trench 2 (Plate 7) measured 4.3 m north/south and was 1.0 m wide at its south end and 1.4 m wide on its north end. It was 0.3 m deep. A 0.2 m deep slot trench, which measured 0.55 m east/west by 1.2 m north/south, was excavated for the bench foundations, approximately 0.9 metres north of the southern end of the trench. A light brown clay deposit [204], possibly natural, was uncovered in the bottom of the slot. A 0.25 m wide concrete foundation [202] for the path [201] ran along the trench's eastern edge. The rest of the trench was filled with a dark brown loam deposit [203], which was not fully excavated.
- 4.1.4 Trench 3 (Plate 8) measured 4.4 metres east/west and was 1.0 m wide at its east end and 1.4 m wide at its west end. It was 0.3 m deep. A 0.2 m deep slot trench, which measured 0.55 m north/south by 1.2 m east/west, was excavated for the bench foundations, approximately 0.9 metres west of the eastern end of the trench. A 0.25 m wide concrete foundation [302] for a path [301] ran along the trench's northern edge. The rest of the trench was covered by a dark brown loam deposit with stone inclusions [303], which was not fully excavated.
- 4.1.5 Trench 4 (Plate 9) measured 4.3 metres east/west and was 1.0 m wide at its east end and 1.4 m wide at its west end. It was 0.3 m deep. A 0.2 m deep slot trench, which measured 0.55 m north/south by 1.2 m east/west, was excavated for the bench foundations approximately 0.9 metres west of the eastern end of the trench. A 0.1 m wide concrete foundation [402] for path [401] ran along the trench's northern edge. A 0.3 m wide gravel deposit [403] (a modern drain fill) was located to the south of the path. A mixed brown loam deposit with stone inclusions [404] covered the rest of the trench.
- 4.1.6 Trench 5 (Plate 10) measured 4.2 metres east/west and was 1.0 m wide at its east end and 1.4 m wide at its west end. It was 0.3 m deep. A 0.2 m deep slot trench, which measured 0.55 m north/south by 1.2 m east/west, was excavated for the bench foundations approximately 0.9 metres west of the eastern end of the trench. A 0.2 m wide concrete foundation [502] for path [501] ran along the trench's northern edge. A dark brown loam deposit with stone inclusions [503] covered the rest of the trench.

4.2 Evaluation

- 4.2.1 Four evaluation trenches were opened to determine the nature of the archaeological remains around the slipway of the Kelvin Dock (Figure 1).
- 4.2.2 Trench 6 (Plate 11) was located on the bottom of a slope on the east side of the Kelvin Dock and measured 4.0 m north/south by 1.6 m wide. It was 1.0 m deep. Three contexts were identified. A cinder-rich deposit [601], which was 0.2 m deep to the north and 0.4 m deep in the south end of the trench, overlay a 0.2 m deep concrete surface [602] which sealed a natural clay deposit [603] (Plate 12).

- 4.2.3 Trench 7 (Plates 13 and 14, Figures 2 and 3) was located in the central part of the slope to the east of the Kelvin Dock and measured 5.6 m east/west. It was between 1.6 m and 2.4 m wide. A depth of 0.05 m of topsoil [701] overlay two dark brown mixed loam deposits [702 and 703], a cinder-rich deposit [704] and concrete foundations [705]. Fittings for rail tracks were identified on the concrete surfaces (Plate 15). The above deposits overlay a natural clay deposit [706].
- 4.2.4 Trench 8 (Plates 16 and 17) was located over a mound near the north wall of the Kelvin Dock and measured 4 m NW/SE by 1.6 m SW/NE. A 0.3 m deep concrete platform [801] overlay concrete foundations [802]. Several fittings were identified on the platform's surface (Plates 18 and 19). A 0.8 m deep deposit of grey gravel and sand made ground [803] underlay the concrete and overlay a black cinder deposit [804] which was up to 0.2 m deep. Below this deposit an orange/light brown loam deposit [805], in the base of the trench, was not fully excavated.
- 4.2.5 Trench 9 (Plates 20 and 21) was located on the upper end of the slope in the vicinity of the gate. It measured 4.5 m north/south by 1.6 m east/west. A cinder-rich deposit [901], which was up to 0.2 m deep, overlay a red gravel deposit [902] that was up to 0.6 m in depth. Another 0.2 m deep cinder deposit [903] underlay this gravel. A rubble deposit [904] was identified at the base of the trench. A docker's hook was recovered from this deposit.

4.3 Historic Building Recording

4.3.1 *The north wall to Skaethorn Road*

- 4.3.1.1 This small east/west stretch of wall consisted of a heavily re-mortared dressed stone rubble wall with a curved cope (Plates 22-24). There was a lot of evidence of brick repair and the wall was in a generally bad state of repair. Towards its east end, where the ground rose to meet Maryhill Road, it terminated in a dressed gate pier with pyramidal cap (Plate 25).

4.3.2 *The east wall to Maryhill Road*

- 4.3.2.1 The east wall to Maryhill Road was a much finer snecked and coursed chisel-dressed wall with rounded cope and swept round towards the former White House to the south of the site (Plates 26-8). There was a small east/west stretch of wall on the inside of this wall, which was in a much cruder stone rubble build (Plate 29) and appeared to be part of a much longer wall.

4.3.3 *Brick and concrete remains* (Figure 4)

- 4.3.3.1 The brick and concrete structural remains comprised two elements: to the north was a concrete platform with numerous reinforced concrete footings (Plate 30) and to the south was a larger brick enclosure with a concrete surface (Plate 31). To the south of the latter feature were four small timber steps.

5 Discussion

- 5.1 The deposits identified during the watching brief consisted of redeposited material and most likely represent made ground related either to the construction of the footpath or the tow-path of the canal. No significant archaeological remains were encountered.

- 5.2 Evidence of several phases of slipway construction was identified during the evaluation. Concrete surface [602] probably represented a slipway structure that was superseded by a slipway constructed of cinder material [901]. This was the latest phase of slipway construction identified during the evaluation. Another concrete slipway element [705], with rail tracks, was identified in Trench 7, again superseded by a cinder slipway structure. The relationship between slipway structure [602] and the tracked concrete [705] is unclear.
- 5.3 A concrete platform was identified on top of the mound in Trench 8. It had fittings on its side but due to its small size its function is uncertain. The higher ground by Trenches 8 and 9 consisted of redeposited material. It was not possible to excavate these trenches to subsoil due to health and safety concerns.
- 5.4 A boundary feature can be identified on the site of the north wall on First Edition Ordnance Survey mapping of the area, produced in the 1850s (Figure 5). It is therefore likely that the north wall was in place by that time. Earlier maps do not show the site in sufficient detail to determine the exact date of its construction. The First Edition mapping also shows a number of buildings (including a police station) at the location of the eastern wall to Maryhill Road. It is possible that the present wall represents the remains of the lock-side wall of these buildings, although there were no features within the wall that identified it as a former building element. These buildings remained standing into the 20th century (the police station was demolished by 1932). By the time of the 1949 Ordnance Survey map of the area, the buildings had been demolished. Brick and concrete features identified during the present works are identifiable on the 1949 map as elements of a slipway adjacent to the graving dock (Figure 6).

6 References

6.1 Bibliographic references

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6.2 Cartographic references

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1857 Ordnance Survey. *Lanarkshire* Sheet 1.13 First Edition. Scale 1: 2,500.

1932 Ordnance Survey. *Lanarkshire* Sheet V1.1 Scale 1:10,560.

1949 Ordnance Survey. Plan 26/5669SW. Scale 1:1250.

Appendix 1

Context Register

Context No	Trench	Description
101	1	Tarmac path surface
102	1	Concrete foundation
103	1	Blaize deposit
104	1	Dark brown loam
201	2	Tarmac path surface
202	2	Concrete foundation
203	2	Dark brown loam
204	2	Light brown clay
301	3	Tarmac path surface
302	3	Concrete foundation
303	3	Dark brown loam
401	4	Tarmac path surface
402	4	Concrete foundation
403	4	Gravel drainage deposit
404	4	Brown loam with stone
501	5	Tarmac path surface
502	5	Concrete foundation
503	5	Dark brown loam
601	6	Cinder deposit
602	6	Concrete surface
603	6	Clay deposit
701	7	Topsoil
702	7	Dark brown loam
703	7	Dark brown loam
704	7	Cinder deposit
705	7	Concrete foundation
706	7	Natural clay deposit
801	8	Concrete platform
802	8	Concrete foundation
803	8	Gravel and sand made ground
804	8	Cinder deposit
805	8	Orange/light brown loam
901	9	Cinder deposit
902	9	Red gravel deposit
903	9	Cinder deposit
904	9	Rubble deposit

Appendix 2

Photographic Register

Black and White Print Film 1

Photo No	Trench	Description	From	Date
1-3	1	Pre-excavation	West	24/03/09
4-6	1	Post-excavation	West	24/03/09
7-9	2	Pre-excavation	East	24/03/09
10-12	2	Post-excavation	West	24/03/09
13	2	Natural clay [204]	NW	24/03/09
14-16	3	Pre-excavation	West	24/03/09
17-19	3	Post-excavation	West	24/03/09
20-22	4	Pre-excavation	SW	24/03/09
23	4	General excavation shot	West	24/03/09
24-26	4	Post-excavation	SW	24/03/09
27-29	5	Pre-excavation	SW	24/03/09
30-32	5	Post-excavation	SW	24/03/09

Black and White Print Film 2

Photo No	Trench	Description	From	Date
1-3	6	Pre-excavation	South	25/03/09
4-6	6	Concrete surface in Trench 6	South	25/03/09
7-9	6	Natural light brown clay	South	25/03/09
10-12	6	Post-excavation view	South	25/03/09
13-15	6	West-facing section of Trench 6	West	25/03/09
16-18	7	Pre-excavation	West	25/03/09
19-21	7	Post-excavation view of Trench 7	East	25/03/09
22-24	7	Post-excavation view of Trench 7	North	25/03/09
25-27	7	Post-excavation view of Trench 7	West	25/03/09
28-30	7	Track fitting in concrete [705]		25/03/09
31-33	7	Location shot	South	25/03/09
34-36	8	Mound near the entrance to the Kelvin Dock, pre-excavation	East	25/03/09

Black and White Print Film 3

Photo No	Trench	Description	From	Date
1-3	8	Post-excavation view	East	25/03/09
4-6	8	South-facing section of Trench 8	South	25/03/09
7-9	8	Concrete slab next to Trench 8	East	25/03/09
10-12	8	Fittings in concrete [801]		25/03/09
13-15	9	Pre-excavation view of Trench 9	South	25/03/09
16-18	9	Red gravel in Trench 9	South	25/03/09
19-21	9	Deposit [903] in Trench 9	South	25/03/09
22-24	9	Made ground (post-excavation view)	South	25/03/09
25-27	9	East-facing section	East	25/03/09

Black and White Print Film 4 (Wall survey)

Photo No	Description	From	Date
1-2	North-facing wall south-facing side – general view	South	26/02/09
3	Detail of corner gate piers	South-west	26/02/09
4	Detail of single gate pier on west side	South-west	26/02/09
5	Detail of concrete platform	North-west	26/02/09
6-7	East wall – west-facing side – general view adjacent to the corner gate piers	North-west	26/02/09
8-10	East wall – west-facing side – general view	North-west	26/02/09
11-12	East wall – detail of the jutting out north/south piece of wall to the South		26/02/09
13	East wall – east-facing side general view to west	North-west	26/02/09
14	East wall – east-facing side general view to west	North	26/02/09
15	East wall – east-facing side general view to west	North-east	26/02/09
16-17	General view of north side of gate piers	North-west	26/02/09
18	North wall of north-facing side	NNE	26/02/09
19	East wall of east-facing side – general view	North-east	26/02/09
20	East wall of east-facing side – general view	South-west	26/02/09

Digital Photographic Register

Photo No	Trench	Description	From	Date
1-3	1	Pre-excavation	West	24/03/09
4-6	1	Post-excavation	West	24/03/09
7-9	2	Pre-excavation	East	24/03/09
10-12	2	Post-excavation	West	24/03/09
13	2	Natural clay [204]	NW	24/03/09
14-16	3	Pre-excavation	West	24/03/09
17-19	3	Post-excavation	West	24/03/09
20-22	4	Pre-excavation	SW	24/03/09
23	4	General excavation shot	West	24/03/09
24-26	4	Post-excavation	SW	24/03/09
27-29	5	Pre-excavation	SW	24/03/09
30-32	5	Post-excavation	SW	24/03/09
33-35	6	Pre-excavation	South	25/03/09
36-38	6	Concrete surface in Trench 6	South	25/03/09
39-41	6	Natural light brown clay	South	25/03/09
42-44	6	post-excavation view	South	25/03/09
45-47	6	W facing section of Trench 6	West	25/03/09
48-49	6	general location view	South	25/03/09
50-52	7	Pre-excavation	West	25/03/09
53-55	7	Post-excavation of Trench 7	East	25/03/09
56-58	7	Post-excavation of Trench 7	North	25/03/09
59-61	7	Post-excavation of Trench 7	West	25/03/09
62-63	7	Track fitting in concrete [705]		25/03/09

64-66	7	Location shot	South	25/03/09
67-69	8	Mound near the entrance to the Kelvin Dock, pre-excavation	East	25/03/09
70-72	8	Trench 8 (post-excavation)	East	25/03/09
73-75	8	South-facing section of Trench 8	South	25/03/09
76-78	8	Concrete slab next to Trench 8	East	25/03/09
79-81	8	Fittings in concrete [801]		25/03/09
82-84	9	Pre-excavation view of Trench 9	South	25/03/09
85-87	9	Red gravel in Trench 9	South	25/03/09
88-90	9	Deposit [903] in Trench 9	South	25/03/09
91-93	9	Made ground (post-excavation)	South	25/03/09
94-96	9	East-facing section	East	25/03/09
97	-	North-facing wall south-facing side – general view	South	26/02/09
98	-	Detail of corner gate piers	South-west	26/02/09
99	-	Detail of single gate pier on west side	South-west	26/02/09
100	-	Detail of concrete platform	North-west	26/02/09
101	-	East wall – west-facing side – general view adjacent to the corner gate piers	North-west	26/02/09
102	-	East wall – west-facing side – general view	North-west	26/02/09
103	-	East wall – detail of the jutting out north/south piece of wall to the South		26/02/09
104	-	East wall – east-facing side general view to west	North-west	26/02/09
105	-	East wall – east-facing side general view to west	North	26/02/09
106	-	East wall – east-facing side general view to west	North-east	26/02/09
107	-	General view of north side of gate piers	North-west	26/02/09
108	-	North wall of north-facing side	NNE	26/02/09
109	-	East wall of east-facing side – general view	North-east	26/02/09
110	-	East wall of east-facing side – general view	South-west	26/02/09

Appendix 3

Discovery and Excavation in Scotland Summary

LOCAL AUTHORITY:	Glasgow
PROJECT TITLE/SITE NAME:	Maryhill Regeneration Stage 3
PROJECT CODE:	AOC 20677
PARISH:	Glasgow (City of Glasgow)
NAME OF CONTRIBUTOR:	Juha Marttila
NAME OF ORGANISATION:	AOC Archaeology Group
TYPE(S) OF PROJECT:	Archaeological Evaluation, Watching Brief and Historic Building Recording
NMRS NO(S):	None
SITE/MONUMENT TYPE(S):	SAM 6773
SIGNIFICANT FINDS:	None
NGR (2 letters, 8 or 10 figures)	Between NS 6189 7270
START DATE (this season)	24 th March 2009
END DATE (this season)	25 th March 2009
PREVIOUS WORK (incl. DES ref.)	None
MAIN (NARRATIVE) DESCRIPTION: (May include information from other fields)	<p>A programme of archaeological works was undertaken by AOC Archaeology including monitoring of ground-breaking works associated with new bench facilities and evaluation and historic building recording in advance of landscaping, as part of the regeneration works at Maryhill Locks, Glasgow, on the Forth and Clyde Canal. The works consisted of the monitoring of five short trenches for bench foundations and the excavation of four evaluation trenches in advance of landscaping works. An historic building recording exercise was also carried out on the northern and eastern boundary walls of the Kelvin Docks and of the upstanding remains in their vicinity in advance of landscaping.</p> <p>Remains of two or three phases of slipway construction, of probable 20th century date, associated with the Kelvin Docks were recorded. A concrete platform was also identified in the northeast corner of the site.</p>
PROPOSED FUTURE WORK:	None
CAPTION(S) FOR ILLUSTRS:	N/A
SPONSOR OR FUNDING BODY:	British Waterways Scotland
ADDRESS OF MAIN CONTRIBUTOR:	AOC Archaeology Group, Edgefield Industrial Estate, Edgefield Road, Loanhead, Midlothian, EH20 9SY.
EMAIL ADDRESS:	juha.marttila@aocarchaeology.com
ARCHIVE LOCATION (intended/deposited)	Archive to be deposited in NMRS

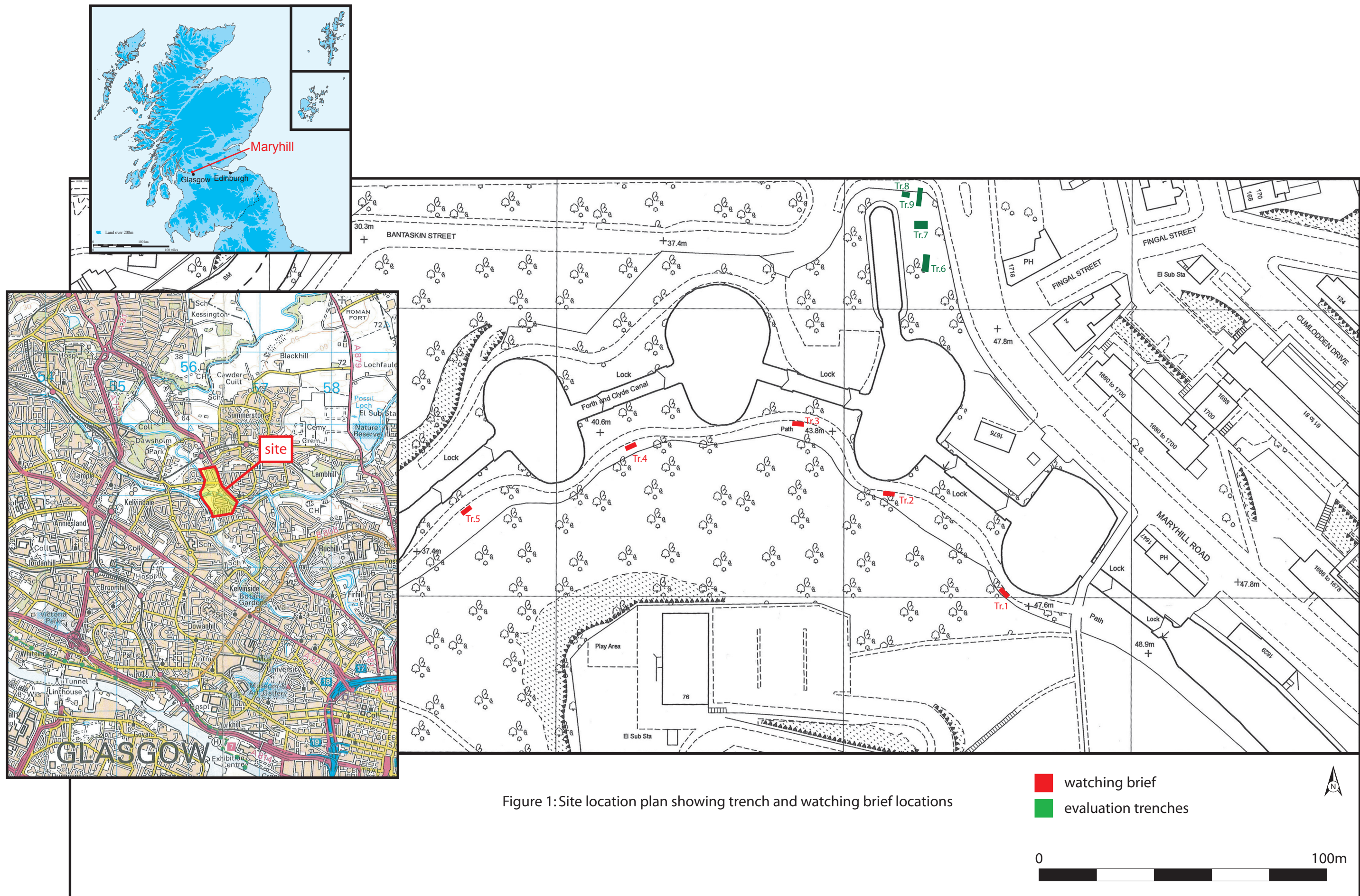


Figure 1: Site location plan showing trench and watching brief locations

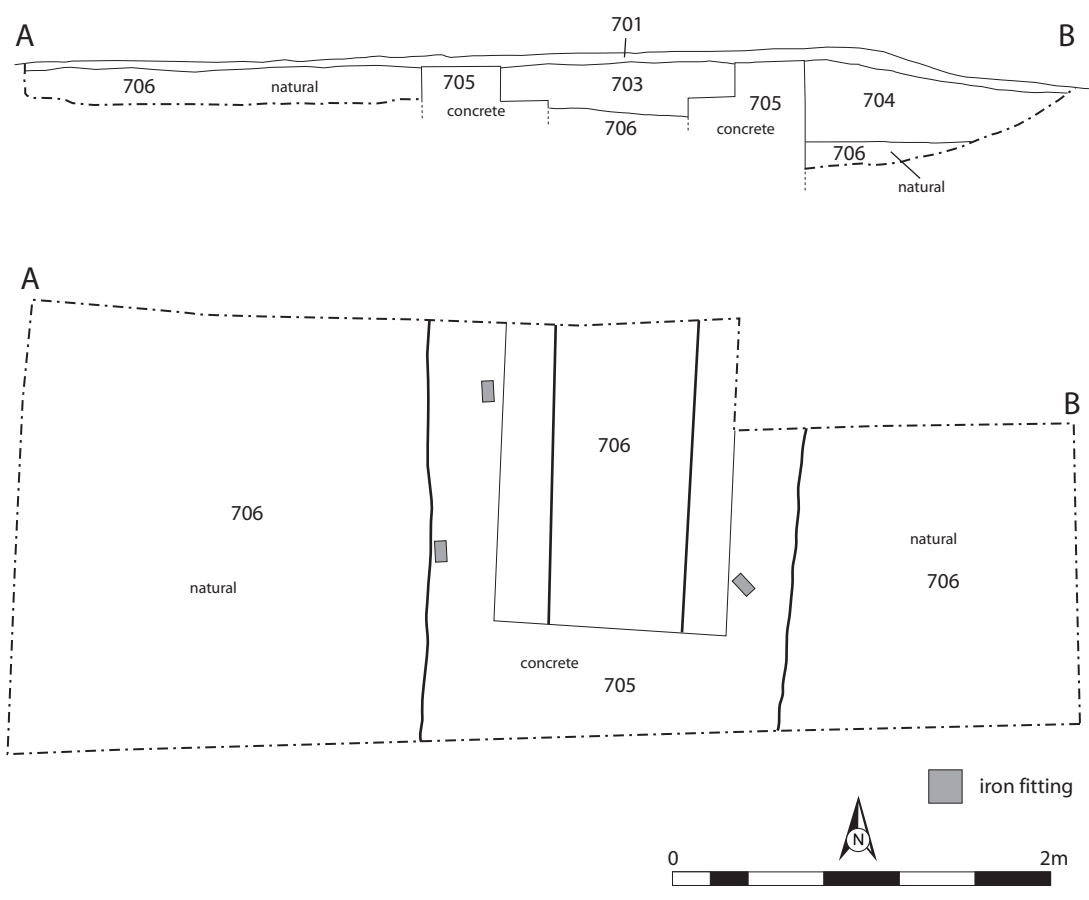
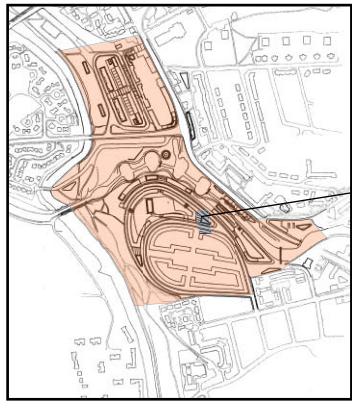


Figure 2: Plan and section of Trench 7



Area covered by Figure 3

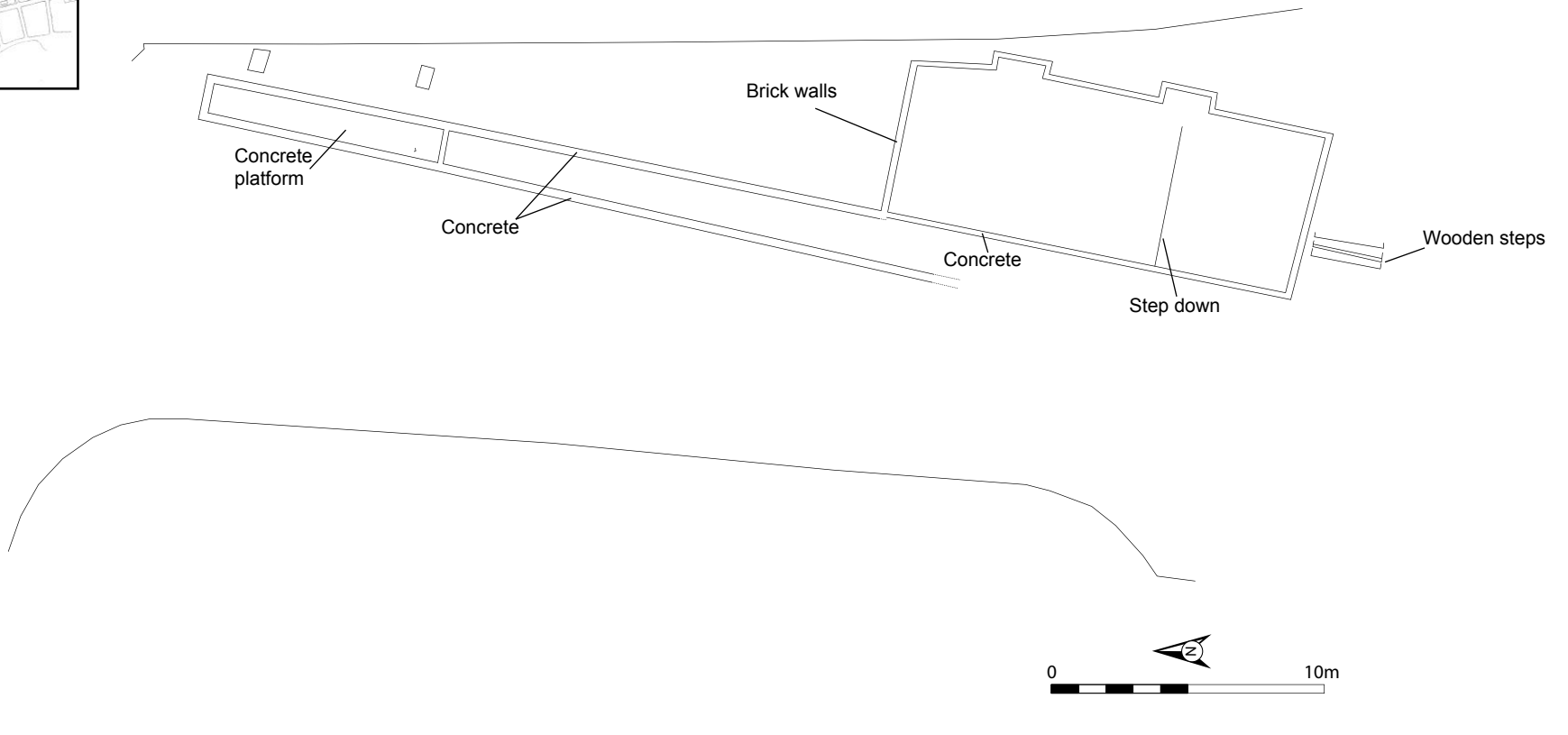


Figure 3: Plan of brick and concrete remains

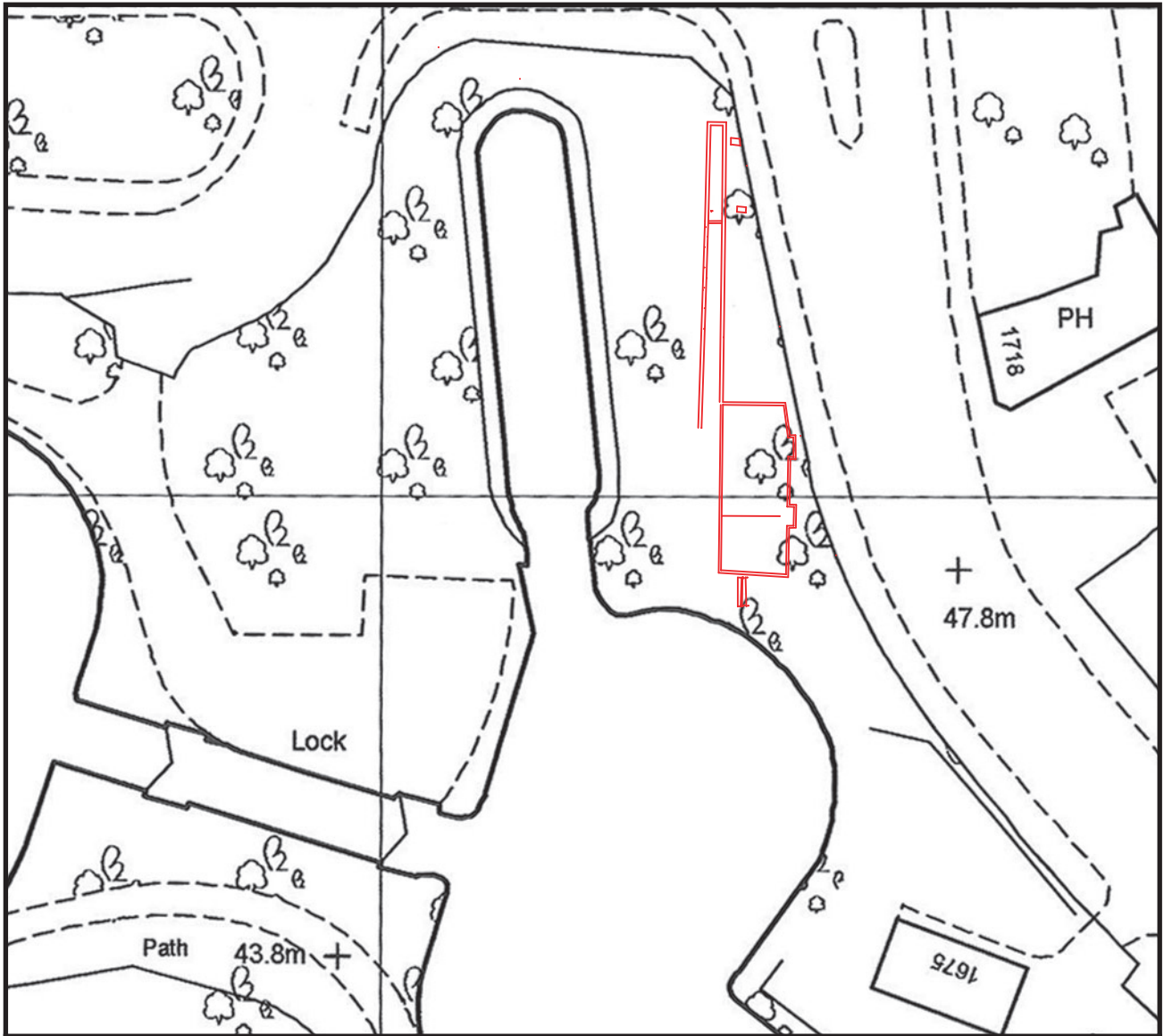


Figure 4: Plan of brick and concrete remains



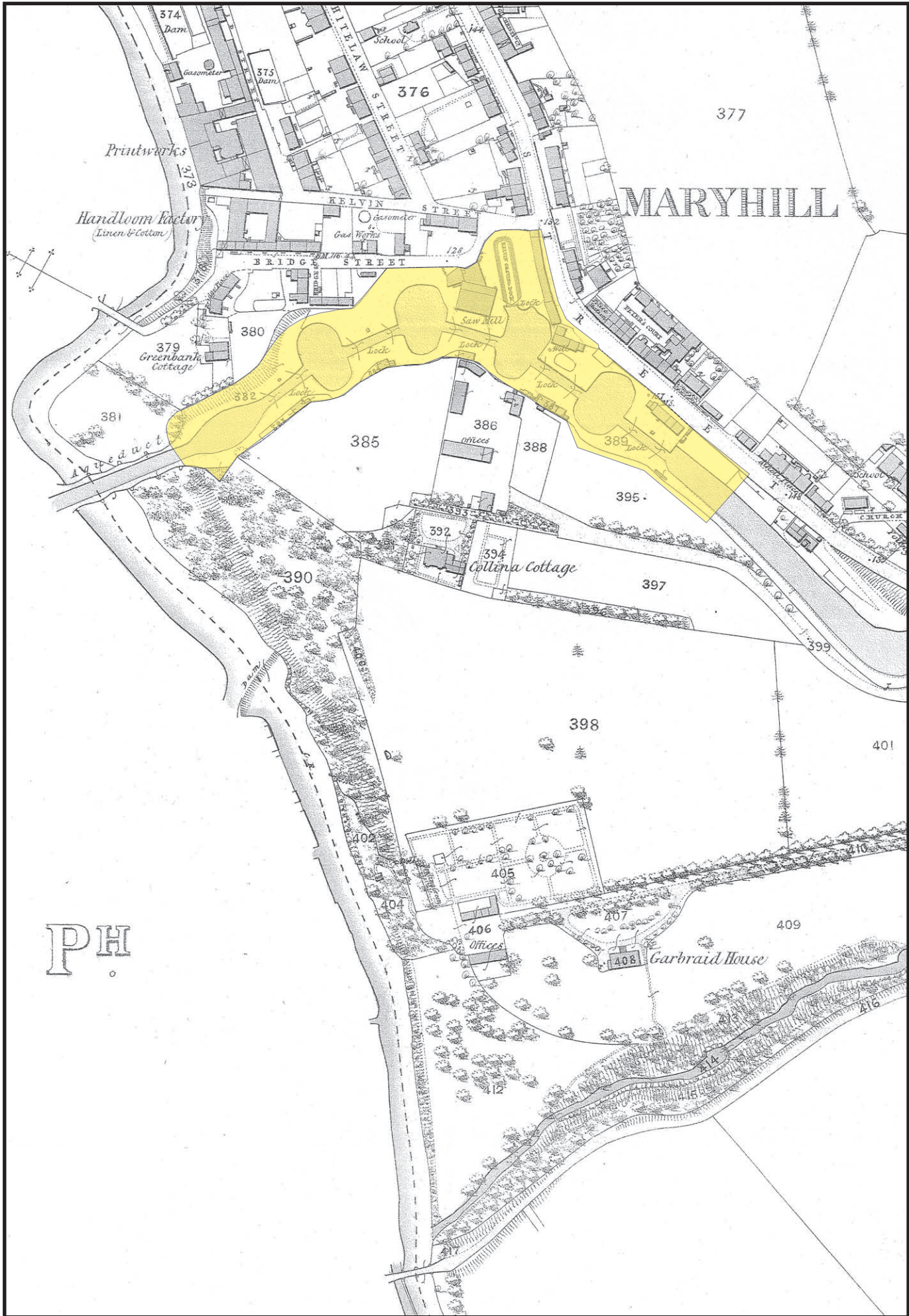
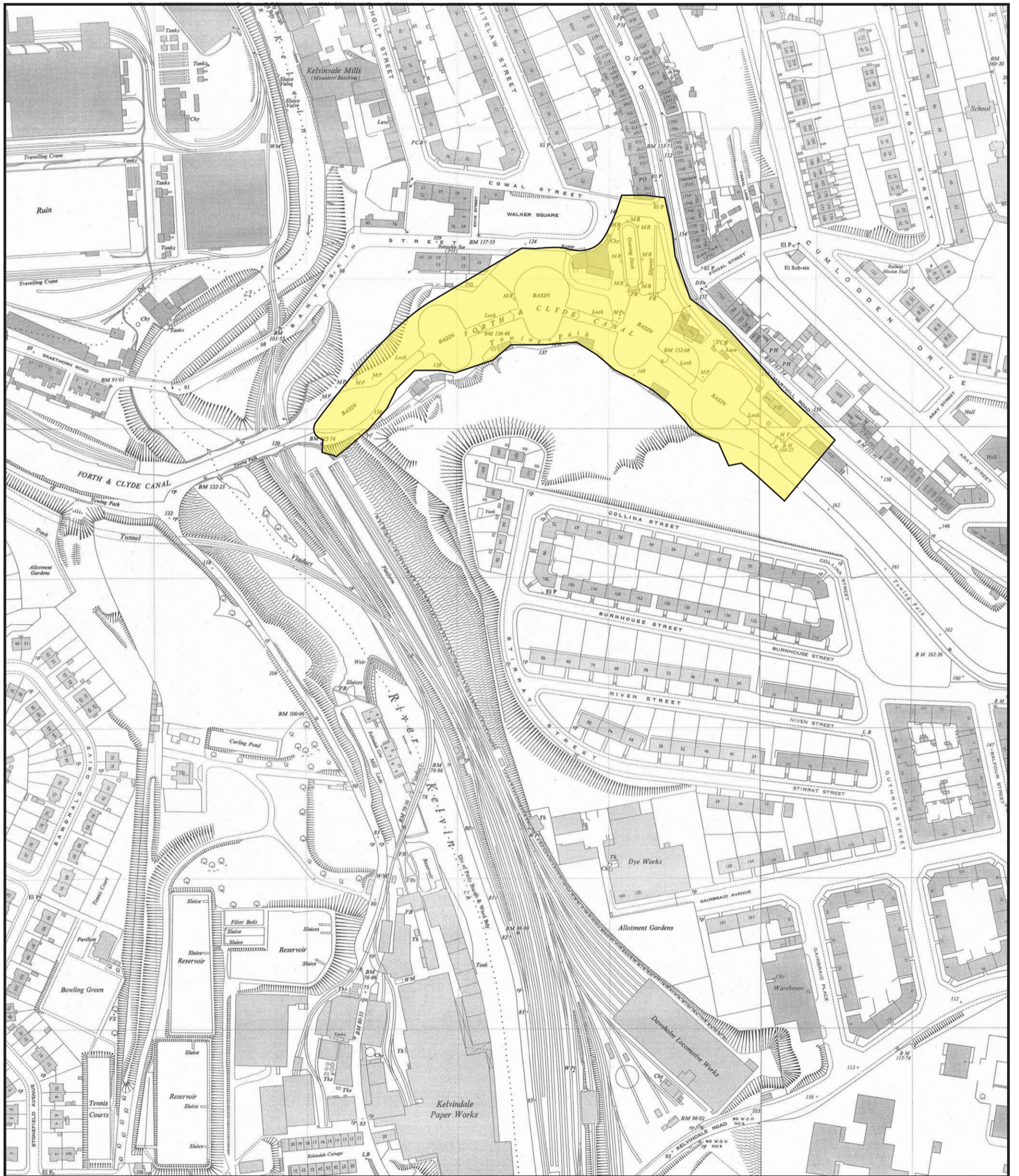


Figure 5: Extract of map by Ordnance Survey, 1857

approximate location of site



■ approximate location of site

Figure 6: Extract of map by Ordnance Survey, 1949



Plate 1: Aerial photograph of the Maryhill Locks



Plate 2: Area of the evaluation from south



Plate 3: Mound from south



Plate 4: The wall N of the Kelvin Dock from SW



Plate 5: The wall N of the Kelvin Dock from SSW



Plate 6: Trench 1 from west



Plate 7: Trench 2 from north



Plate 8: Trench 3 from west



Plate 9: Trench 4 from west



Plate 10: Trench 5 from west



Plate 11: Trench 6 from south



Plate 12: Trench 6 from west



Plate 13: Trench 7 from east



Plate 14: Trench 7 from north



Plate 15: Rail track in concrete surface, Trench 7



Plate 16: Trench 8 from east



Plate 17: Trench 8 from south



Plate 18: Concrete platform adjacent to Trench 8 from east



Plate 19: Fittings on platform adjacent to Trench 8



Plate 20: Trench 9 from south



Plate 21: East facing section of Trench 9



Plate 22: General view of north wall to Skaethorn Road from south



Plate 23: General view of the north wall to Skaethorn Road from NW



Plate 24: General view of the north wall to Skaethorn Road from NE



Plate 25: Detail of the gate pier between the northern and eastern walls from south



Plate 26: General view of the eastern wall to Maryhill Road from NW



Plate 27: General view of the eastern wall to Maryhill Road from NW



Plate 28: General view of the eastern wall to Maryhill Road from NNE



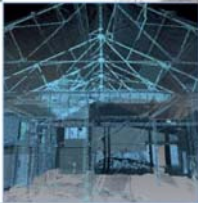
Plate 29: Detail of the stretch of wall on southern side of eastern wall to Maryhill Road



Plate 30: General view of the northern side of the slipway, showing concrete footings and associated fittings from NW



Plate 31: General view of the brick enclosure associated with the slipway from NW



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